

Hatcher Road Revitalization Plan



Prepared by the Ecosa Institute

For the Hatcher Road Committee

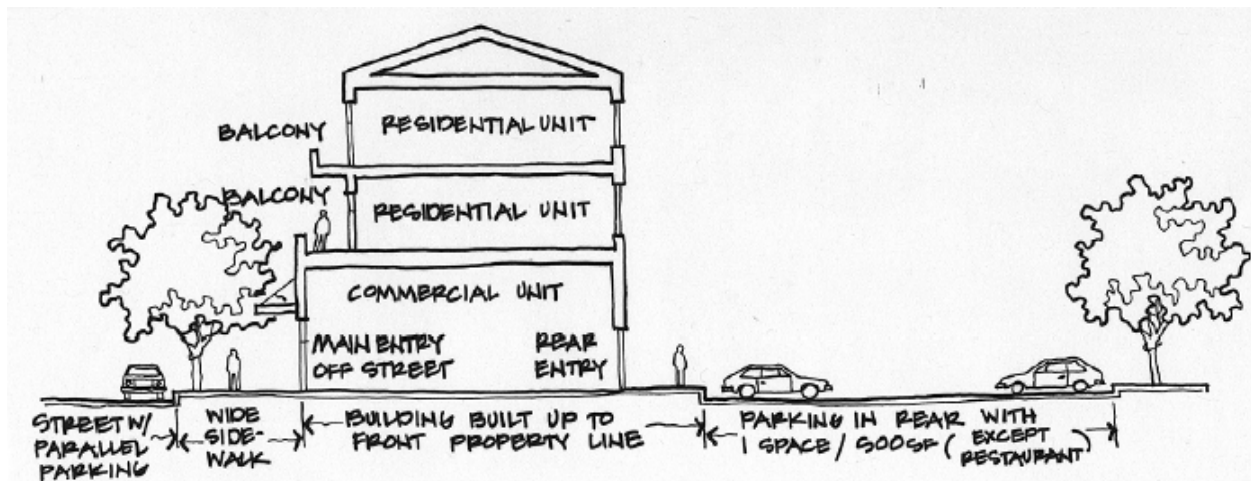
Goal:

The goal of this document is to be a guide for future revitalization efforts and to promote the livability and identity of the neighborhood by promoting a mix of residential, commercial, office, and civic development along Hatcher Road.

Hatcher Road and the greater Sunnyslope community have been and still are a home to thousands of people who are proud of their lives and experiences in and around Sunnyslope. These residents often feel unable to express their appreciation for Sunnyslope based on its reputation. To change this, the Sunnyslope community wants to turn Hatcher road into a destination. The residents would like Hatcher road to be a pleasant pedestrian oriented multi use shopping and dining area. Currently Hatcher road is an automobile dominated collector street that is well on its way to becoming a transit artery. Creating a pedestrian destination on Hatcher road will not only give Sunnyslope residents a sense of community pride, but it will also improve public safety, reduce neighborhood cut-through traffic, and improve business.

Throughout the redevelopment planning process, stakeholders from the residential and commercial areas around Hatcher road expressed interest in seeing new development and redevelopment along the area's commercial corridors be mixed use. Mixed use developments refers to developments on a single parcel or within a master plan development which contains some mix of uses, such as residential, office, retail, commercial, institutional. Mixed-use developments allow for street level retail close to the sidewalk and residential uses on upper floors.

The above figure illustrates a proposed traditional Mixed-used development with commercial units on the ground floor, residential units above, and parking to the back of the building.



The following photographs are examples of mixed-use buildings. On the left is a photograph of a recent mixed-use development, Jefferson Center, located in northwest Austin TX.

On the right is the Belmont Dairy redevelopment in Portland OR.



Hatcher Road Proposed Objectives

- Objective 1: Promoting a mix of residential, commercial, office, and civic development in close proximity to Hatcher Road, thereby creating pedestrian friendly activity and minimizing traffic congestion
- Objective 2: Promoting the livability and identity of the neighborhood by providing for dwellings, shops, and workplaces in close proximity to one another, thereby decreasing dependence on the automobile
- Objective 3: Implementing design standards to create the character, scale and architecture of the Hatcher Road's commercial areas; encouraging development of attractive, convenient, and pedestrian-friendly off-street parking facilities
- Objective 4: Development projects should serve the local community while also becoming a shopping or dining destination for the rest of the city

These goals are designed primarily for future developments but also benefit the existing businesses and residents along Hatcher Road by mitigating the effect of congestion and pollution by creating accessible neighborhoods and by enhancing the economic stability of Hatcher Road by promoting the attractiveness, convenience, and accessibility of the areas to which it applies.

This will be accomplished by:

- Concentrate commercial development in a mixed-use environment
- Encourage pedestrian-oriented design of new facilities
- Enhance the efficient usage of parking (shared parking)
- Protect and enhance visual quality
- Create public open space

Mixed Use:

Recommendation 1

Require mixed use buildings for all new development in commercial zoning along Hatcher road.

Recommendation 2

Allow mixed use buildings in lots zoned for R5 along Hatcher road

Height requirements:

Recommendation 1

Development on the north side of Hatcher road between 7th Ave and 2nd St should restrict building height to one story tall, so as to preserve vision of the S on the northern hill. The southern side of Hatcher road should have a 40 foot height, encouraging densification while protecting the skyline of the neighborhood. Limiting the height to 40 feet will avoid creating a canyon like effect along Hatcher road.

Recommendation 2

New buildings should be built no higher than 1 story taller than adjacent buildings to prevent developments from towering over the adjacent single family neighborhoods.

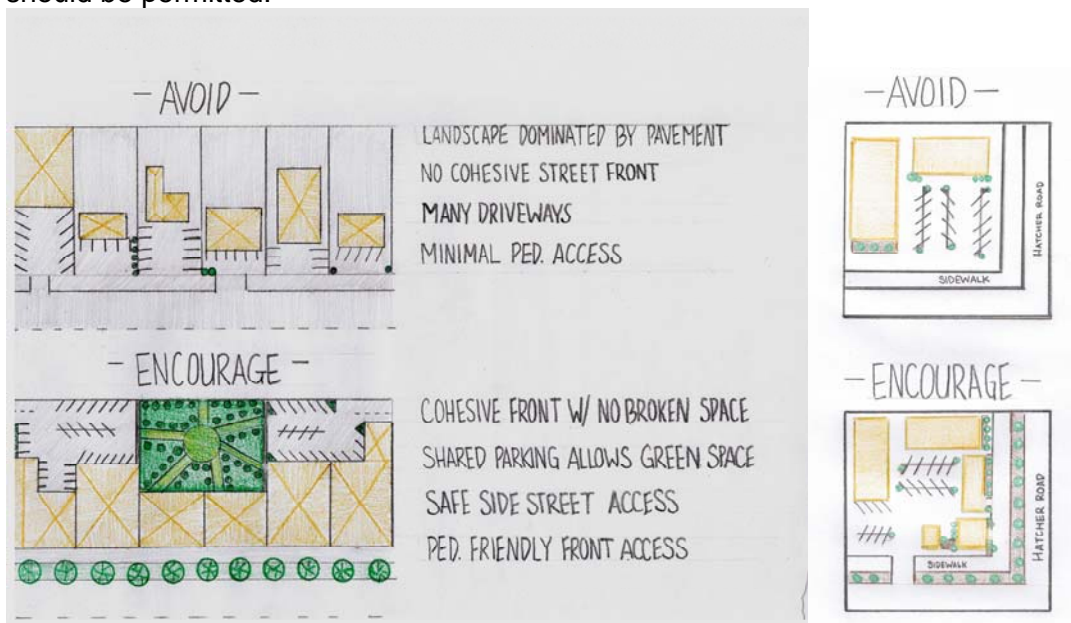
Parking:

Recommendation 1

Moving parking to the back of the lots in new developments should help create a pedestrian friendly space.

Recommendation 2

Driveways should be shared between neighboring lots to rear parking when possible. The amount of space saved on each lot would then be available for development thus increasing the lot's build-able area. The minimization of driveways increases traffic predictability, traffic safety, and pedestrian safety. Existing curb cuts should be closed where possible. No new curb cuts should be permitted.



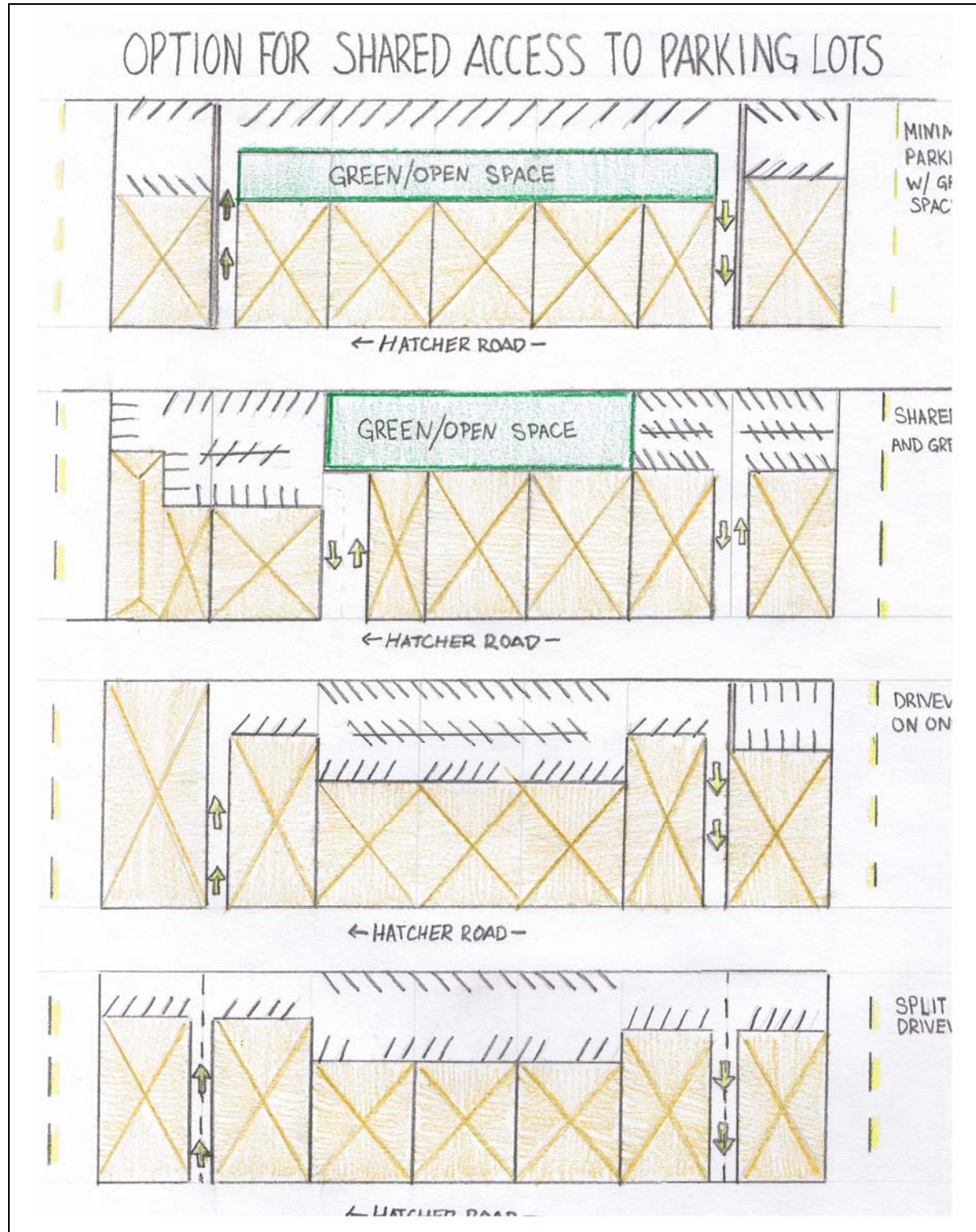
Above left is a diagram of ideal communal parking for a city block
Above right is a diagram of ideal parking for multi lot developments

Recommendation 3

Parking requirements may be satisfied by leasing parking spaces within 900 feet of development.

Recommendation 4

Parking requirements for mixed use buildings are set to the larger parking requirements for the commercial or the residential uses or 30% less than their combined parking requirements. Since the parking spaces are typically occupied at alternating times of day, providing parking for both uses simultaneously is necessary.



Recommendation 5

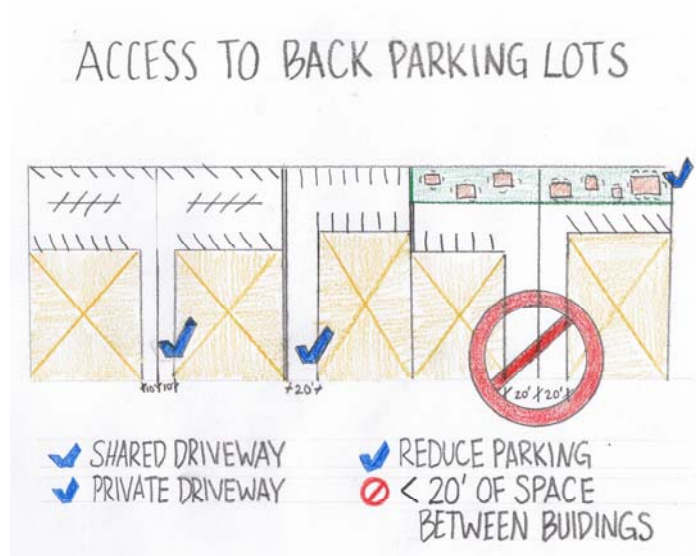
Parking requirements for commercial developments should be reduced by 10 percent if development is located within 900 feet of non-residential public parking.

Recommendation 6

Surface parking lots should be discouraged. Parking for multi-family projects and mixed-use developments should be located either underground or in structured parking garages not to exceed 20 percent of the lot coverage, level leaving the 80 percent lot coverage for commercial office and retail space. Such parking structures should be properly incentivized to encourage their development. Parking garages should be designed using flat slabs to enable the conversion of the garage to residential uses in future, should alternative transportation choices reduce demand for the facility.

Recommendation 7

All surface parking lots should be designed with porous or pervious surfaces. These surfaces reduce the volume and rate of storm water runoff, increase the available ground water, and add to the visual character of the parking lot.



Recommendation 8

On-street parallel parking spaces shall have a dimension of 8 feet by 20 feet. On-street angled parking shall be at a 45 degree angle. Angled parking spaces shall have a depth of 18', and a width of 9'. A minimum distance of 15 feet is required from the crosswalk or stop sign line to the first parking space.

Recommendation 9

Pedestrian walkways should be buffered from surface parking by plantings, low walls or low fences.

Recommendation 10

Surface parking lots should not exceed 20% percent lot coverage.

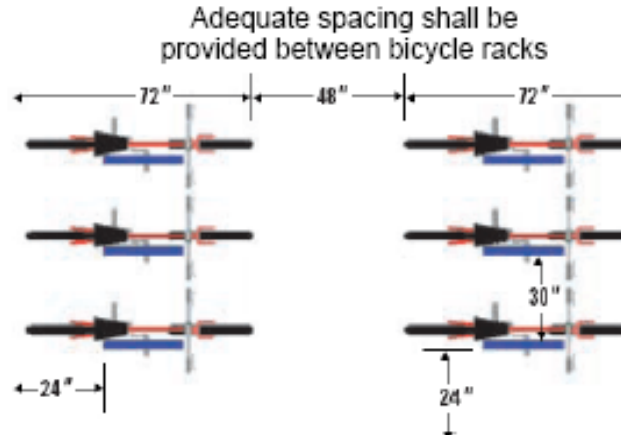
Bicycles:

Recommendation 1

Bicycle parking place or lock-station should be provided for one fifth the anticipated building occupancy.

Recommendation 2

Showers are required for businesses employing 10 or more people. One additional shower should be installed for every 50 additional employees.



Recommendation 3

Inverted “U” racks mounted in a row should be placed 30 inches apart (on center) allowing enough room for two bicycles to be secured to each rack and providing easy access to each bicycle.

Recommendation 4

The rack should be anchored so that it cannot be stolen with the bike attached. Racks that are large and heavy enough such that the rack cannot be easily moved or lifted with the bicycles attached do not have to be anchored. Bicycle racks should resist being cut or detached using common hand tools, such as bolt cutters, pipe cutters, wrenches, and pry bars.



Example of creative bicycle rack

Recommendation 5

Bicycle racks may be placed on private property and public sidewalks. In both cases, the racks shall not be placed so that they block the entrance or inhibit pedestrian flow in or out of the building. If placed on a sidewalk or pedestrian walkway, they should be placed so that at least 4 feet of walkway is maintained.

Recommendation 6

Racks shall be mounted within 50 feet of the entrance it serves, or as close as the nearest car parking space, whichever is closer.

Pedestrians:

Recommendation 1
Pedestrian oriented lighting should be installed in all parking lots and along all walkways.



Recommendation 2
Limit automobile oriented development to promote a more pedestrian-friendly area. This can be accomplished by encouraging pedestrian oriented developments such as the one pictured to the right.

Recommendation 3
Streetscape furniture and amenities shall be located to maintain a clear pedestrian path of at least four feet in width.

Recommendation 4
Pedestrian lights should be no taller than 14 feet tall so the light is not blocked by tree foliage.



Recommendation 5
A consistent tree canopy should be established to promote the walk-ability of Hatcher road.



Planting:

The goal of this section is to create a pleasant, shaded streetscape that encourages walking, reduces the heat island effect, and employs on-site rainwater for all irrigation. To ensure full mature tree canopies without long term additional watering, plant selection and planter specifications are crucial.



Recommendation 1
Drought tolerant shade trees and other native plants should be planted in all publicity accessible areas.



Recommendation 2

The following private property areas shall be landscaped and regularly maintained to be free of weeds, overgrown vegetation, and litter:

- *Unpaved portions of the site visible from public streets, sidewalks, plazas, parks, and other public spaces.
- *Common outdoor areas within any development
- *Private and Public surface parking lots.

Recommendation 3

Landscaping should include any of the following Phoenix native drought tolerant species: Beargrass, Blackfoot daisy, Blue Palo Verde, Desert marigold, Desert Willow, Fairy Duster, Hopbush, Little-leaf-cordia, Ocotillo, Soap-tree yucca, and Sweet acacia (all pictured above).

Recommendation 4

Other drought tolerate plantings may be approved by the Phoenix Planning Commission.



Recommendation 5

Roadside planters should be designed to harvest their own rainwater thus eliminating the need for long term irrigation. This can be achieved with 'curb cuts.' By removing a short section of the roadside curb on the uphill side of each planter, and by having each planter sunken rather than raised, the planters can collect all the water they will need to support drought tolerant plants (as seen in the figures above).

Recommendation 6

To provide adequate pedestrian clearance, trees should be pruned regularly so that there is at least 7 feet of vertical clearance between the lowest branches of the tree and the grade of the adjacent sidewalk. They shall also be pruned to maintain the health, vigor, and natural shape of the tree.

Recommendation 7

Planter surfaces should be recessed by 1 foot and filled in with mulch. This encourages water retention and infiltration.

Building Façades:

Keeping the façade interesting

Recommendation 1

Façades must be designed with both solid surfaces and window openings to avoid the creation of blank walls and glass curtain walls

Recommendation 2

A clear visual distinction between each floor must be provided on the building façade

Recommendation 3

Street facing façades should be articulated to improve the quality of design. This can be achieved by:

- *creating a defined roof-line
- *providing stylized windows and doors
- *creating a defined base for the building
- *providing three-dimensional expression lines (vertical and horizontal) between the floors of the structure and around storefronts and window openings
- *Adding depth and detail to the cornice or roof parapet
- *Recessing storefronts and windows into the façade to create depth and cast shadow patterns.

Consistent design standards

Recommendation 4

Stairways should be designed to be aesthetically integrated with the building.

Recommendation 5

If a portion of an existing building is modified, it should use similar building materials, details and paints as the rest of the building.

Recommendation 6

Use no more than 3 different types of materials on a single façade.

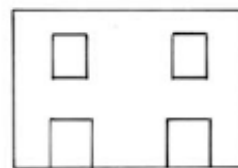
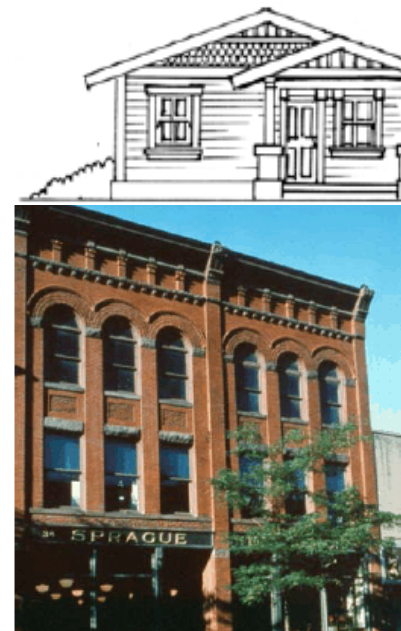
Recommendation 7

Materials should be consistent with local vernacular architecture, or those that are indigenous to the region.

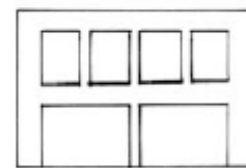
Recommendation 8

Building materials that are discouraged on façades include: plywood, hardboard, corrugated fiberglass, vinyl or lap aluminum siding, and sheet metal.

Original Structure:



Unarticulated
Façade



Façade articulation is improved
by increasing the number and
size of window openings



Façade articulation is improved
by adding a defined building
cap or roofline with depth and
detail, and by adding stylized
windows and doors



Façade articulation is
improved by providing a
building base and three-
dimensional horizontal and
vertical expression lines

Recommendation 9

Changes in material should generally occur when there is a change in the plane of the facade.

Recommendation 10

Painted building surfaces should have a matte finish. Trim work may have a glossy finish. Consideration should be given to how certain colors and finishes withstand weather.

Recommendation 11

The natural colors of brick and stone material should be maintained. These materials should not be painted or glazed.

Recommendation 12

Fluorescent and neon colors should be avoided. If used, fluorescent and neon colors should only be used as accent colors on window and door frames.

Recommendation 13

Commercial building facades facing pedestrian walkways should be designed with ground floor storefront windows and doors with clear glass to provide clear views into and out of the storefronts. Mirrors should be discouraged.

Recommendation 14

The use of traditional “Main Street” storefront elements including recessed entry doors, display windows, kick-plates, transom windows, cornice and pediment are strongly encouraged.

Recommendation 15

Secondary entrances and windows are strongly encouraged on rear facades that are adjacent to parking facilities.

Recommendation 16

Storefronts should be recessed into the wall plane by at least 6 inches and be framed so as to give the pedestrian a feeling of space.

Recommendation 17

Overhanging roofs or three-dimensional cornices are recommended to delineate the top of the facade and cast shadows furthering the visual articulation of the facade

Recommendation 18

Mechanical equipment on roofs shall be screened and shaded to protect public views and increase equipment efficiency

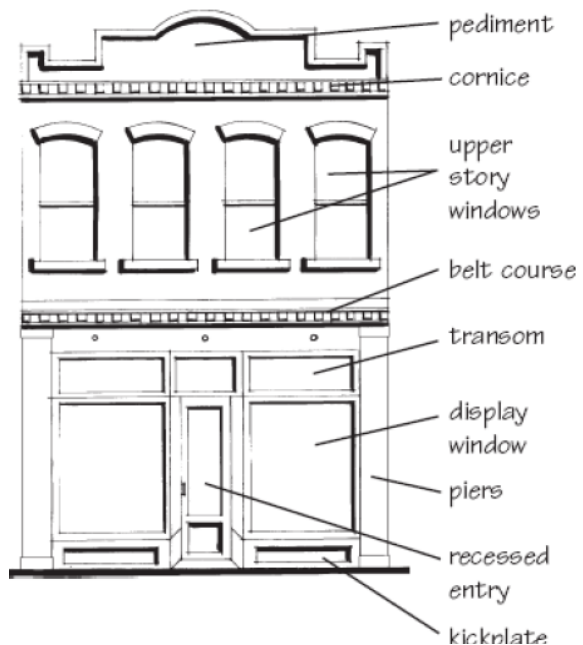
Recommendation 19

Awnings and marquees should not exceed 18 inches in height.

Prohibited Addition to Structure:



Traditional Main Street Storefronts



Recommendation 20

Awning coverings should be made of canvas or fabric

Recommendation 21

Awnings should cover individual windows or entrances rather than entire storefronts. All such awnings should have a consistent design and color.

Balconies, Colonnades, Porches and Stoops

Recommendation 22

Balconies, including railings, columns, details, and roof covering should be compatible with the overall style and form of the building. The balcony should not look like an addition or afterthought.

Recommendation 23

Colonnades may be covered with a roof or a balcony. An enclosed habitable space may occur above the colonnade, as long as it does not occur over the public sidewalk.

Recommendation 24

If used, colonnades should extend along the entire width of the building facade. If building is on a corner lot, the colonnade may turn the corner and extend along the side facade.

Recommendation 25

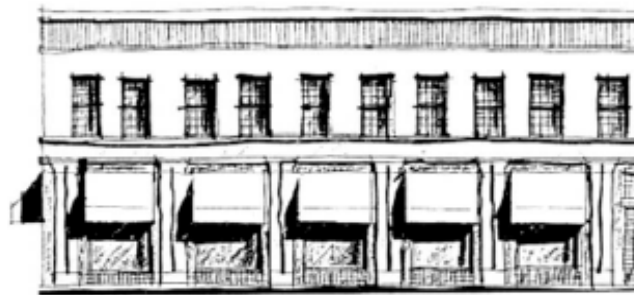
The size of columns or posts used to support the colonnade covering should be equal in size and uniform in appearance.

Recommendation 26

Residential buildings are encouraged to have front porches and stoops.

Recommendation 27

Front porches or stoops may be covered with a roof, a balcony, or an enclosed habitable space.



Multiple awnings are located within the structural bays of storefronts (Encouraged)



Plazas

Recommendation 28

All plazas should be designed with an ornamental surface that is differentiated from the sidewalk and asphalt streets and parking lots.

Recommendation 29

All plazas and courtyards shall be designed with pedestrian amenities, such as seating, outdoor dining tables with umbrellas, planters, trees, lighting, and public artwork.

Recommendation 30

A combination of hard and soft surfaces shall be incorporated into the design of plazas and courtyards to add visual interest and variety.

Public, open, and visible undeveloped space

Recommendation 31

Public and open spaces should be provided with:

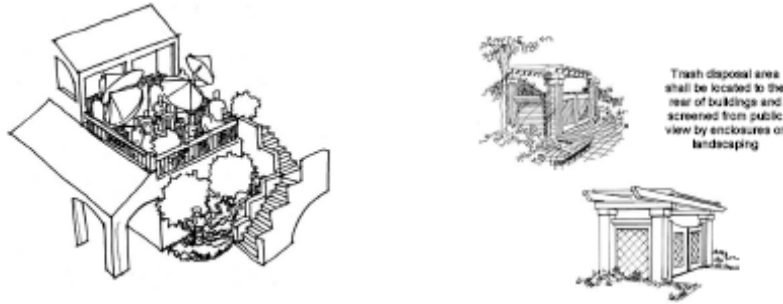
- *Trash and recycling receptacles
- *A combination of landscaping and paved surfaces
- *Amenities or features that encourage people to gather such as outdoor seating planters or barbeque facilities

Recommendation 32

Trash disposal areas should be screened or otherwise protected from pedestrian view.

Recommendation 33

Mechanical equipment including heating and cooling equipment as well as utility meters and satellite dishes greater than 18" in diameter should be protected from public view.



Fences and perimeter walls

Recommendation 34

Fences should be aesthetically compatible with the architecture of the building and built out of wood, stone, iron, brick or other durable and aesthetically comparable materials.

Recommendation 35

Fences and exterior separation walls should have an articulated design. Articulation can be created by having regularly spaced posts, changing the height of the fence, and by using different building materials at the base, posts, or the cap of the fence/wall. Flat walls, chain link fences, and barbed wire fences are strongly discouraged.

Recommendation 36

The maximum fence/wall height should be 4 feet within 30 feet of the front property line, and 6 feet beyond 30 feet of the rear property line.

Outdoor lighting

Recommendation 37

Sign lighting should be located and directed to light the intended area of illumination only and to prevent off site glare impacts on any adjacent buildings or properties.

Recommendation 38

Over-lighting of buildings and sites should be avoided. Over-lighting can create an unpleasant and harsh environment, which can also prevent pedestrian vision outside of the lighted area, creating pedestrian insecurity.

Examples of safe and desirable night-time lighting:



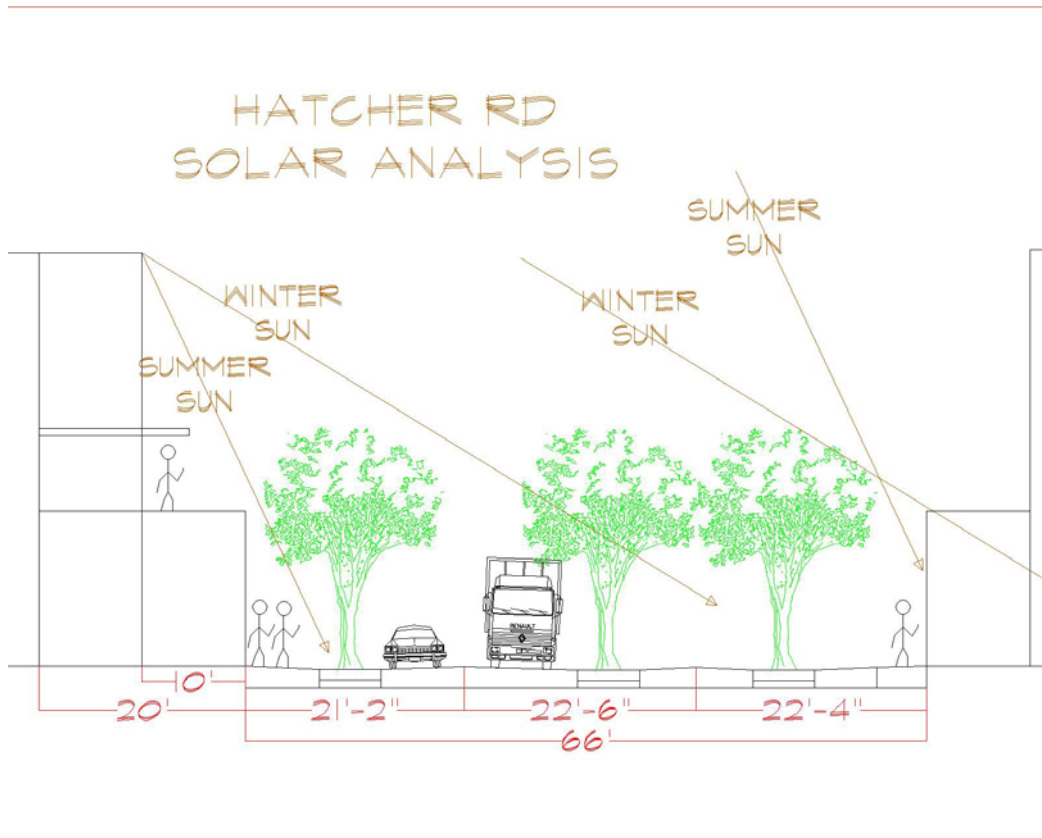
Recommendation 39

All lighting poles and fixtures should have a decorative/ornamental design that complements the structures on the site.

Setbacks:

Recommendation 1

Minimum setback is zero feet or directly on the lot lines. The maximum Setback is ten feet. If the ground floor is placed directly on the lot line, the second floor (or any part of the structure extending above 13 feet) should be set back by eight to twelve feet, creating a pleasant aesthetic, and creating a place for a second floor patio or balcony. This will also create shade over the southern sidewalk during summer, and ensure solar access for the northern sidewalks during the winter months.

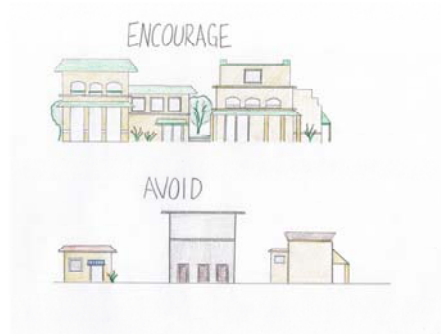


Recommendation 2

Exemption from the ten foot maximum can be applied for, if public amenities like seating or plazas are being provided. In this case the maximum setback would be 20 feet.

Recommendation 3

Decrease the side setbacks, encouraging buildings to be placed closer to each-other and oriented towards the street which both improves the pedestrian experience and allows for a reduction in heating and cooling loads due to the solar benefits of such positioning.



Recommendation 4

Front setback area may have a maximum of 50% paving, the remainder to be landscaped. If this area is to be used for café or restaurant seating paving may be a maximum of 75%.

General Recommendations

Recommendation 5

Commercial development on Hatcher road should provide shopping and services for the nearby neighborhoods as well as the rest of the city.

Recommendation 6

Development should be centered on larger intersections thereby creating nodes.

Recommendation 13

The commercial node centered on the intersection of Hatcher and Central should be redeveloped to become pedestrian friendly